



**MEATH COUNTY
COUNCIL**
Proposed Library
Development, Bettystown, Co.
Meath
Outline Construction Traffic
Management Plan
(CTMP01)

June 2020



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For and on behalf of
MPA Consulting Engineers

DOCUMENT CONTROL SHEET

	Client	Meath County Council							
	Project Title	Proposed Library Development, Bettystown							
	Project Ref.	191025							
	Document Title	Outline Construction Traffic Management Plan							
	Document No.	191018-CTMP-001							
	This Document Comprises	DCS	TOC	Text	LOT	LOG	LOF	LOD	Appendices
		1	1	15	-	-	-	-	1
Check									

Revision	Status	Author	Reviewed By	Approved By	Issue Dates
D01	Draft Issue	JA	MJP	MJP	29/04/2020
F01	Final	JA	MJP	MJP	25/06/2020

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1.0 INTRODUCTION

1.1 General

- 1.1.1 This outline Construction Traffic Management Plan (CTMP) has been prepared by MPA Consulting Engineers at the request of our client, Meath County Council, who are proposing to construct a three-storey library and lifeguard station development in Bettystown, County Meath.
- 1.1.2 This CTMP is submitted to allow a thorough assessment of potential construction traffic related issues however, it should be noted that full details of the construction programme, the source / routing of materials and the number of construction traffic movements is not yet known. This report therefore sets out the best available information at this time.
- 1.1.3 As a consequence, this report is presented as an 'Outline' version to identify and establish certain key principles concerning construction traffic movements. It is recommended that a planning condition is included in the permit requiring submission and approval of a more detailed version prior to commencement on site.

1.2 Scope

- 1.2.1 This CTMP considers the movement of material and personnel associated with the construction phase of the proposed development. The objective of this outline report is to manage and mitigate the impacts of construction traffic on the public road network.
- 1.2.2 Parts of the construction process will require the movement of earth works and construction material to and from the development site by Heavy Goods Vehicle (HGVs). This outline CTMP describes how these HGV movements will be managed during the various stages of construction as well as the interaction with the surrounding road network.
- 1.2.3 The Plan also considers the vehicle movements associated with the workforce involved in the construction as they travel to and from the site.
- 1.2.4 This Plan should serve as the basis for the final CTMP with the appointed construction contractor required to prepare the final version once all programme, material quantity and material source information is known.

1.3 Structure of the Report

- 1.3.1 The remainder of this document is structured as follows:
- Section 2 provides an overview of the proposed development and the draft construction programme together with details of the site access arrangements for construction related traffic.
 - Section 3 identifies the construction traffic generation and staff levels.
 - Section 4 identifies the measures to be introduced to manage construction traffic.

- Section 5 identifies the communication, emergency procedures and enforcement of the plan.
- Section 6 outlines the conclusions of this report.

2.0 PROPOSED DEVELOPMENT

2.1 General

- 2.1.1 This section summarises the proposed development, access arrangements for construction traffic and the expected length of the construction programme.
- 2.1.2 The development site is located on the north-east corner of the R150 and Seaview Terrace in Bettystown, County Meath. The site surrounded by a mix of retail, food and drink and residential premises. Bettystown foreshore abuts the site's eastern boundary.
- 2.1.3 The R150 and R151 are the main access roads to site. The R150 connects south from Drogheda and further extends south along the coast before connecting with the R132 in Julianstown. The R151 (Golf Links Road) extends north from the town centre to Mornington and Drogheda.
- 2.1.4 The M1 (Dublin – Belfast) is located approximately 10km south-west of the site and is accessed via the M1 / R132 (Gormanstown) interchange.
- 2.1.5 The site location in the context of Bettystown and the connecting road network is shown in **Figure 2.1, Figure 2.2 and Figure 2.3**

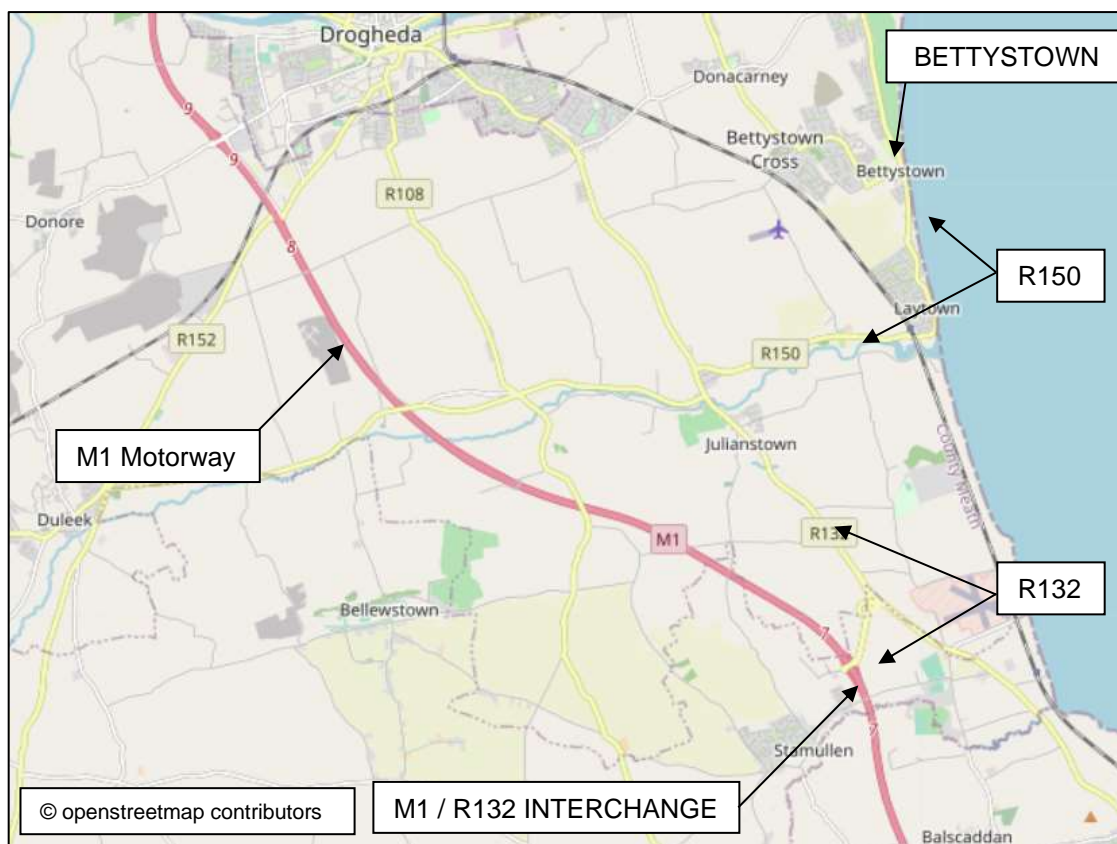


FIGURE 2.1: M1 CONNECTION TO BETTYSTOWN



FIGURE 2.2: SITE LOCATION WITHIN BETTYSTOWN

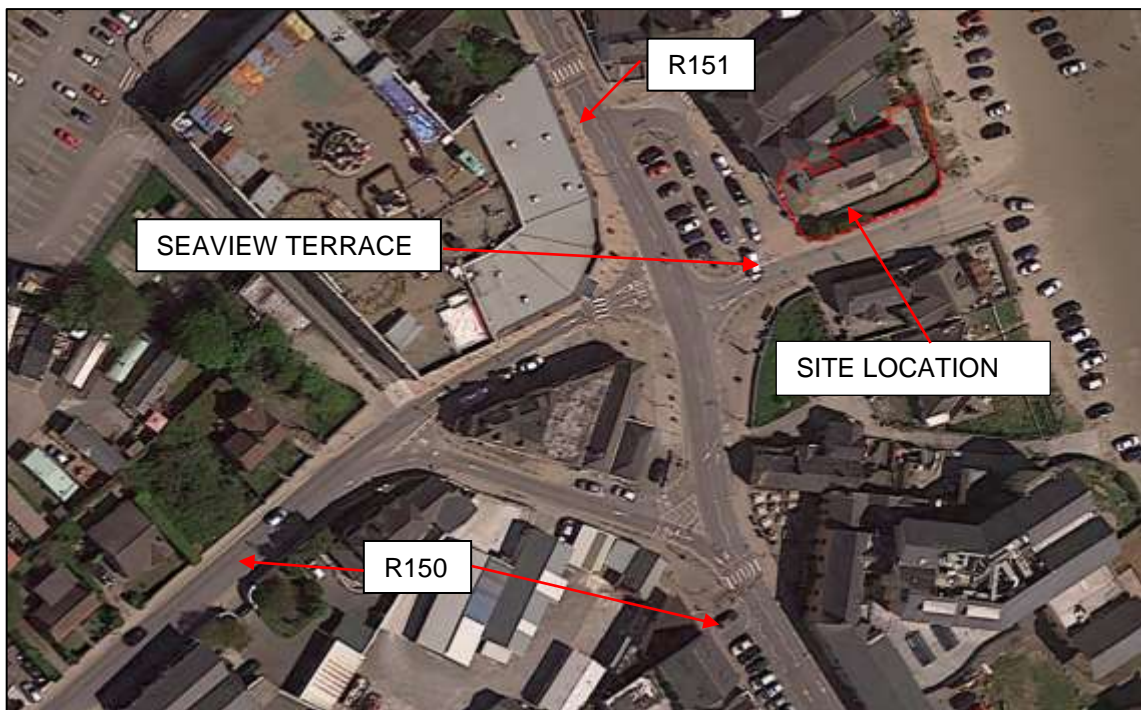


FIGURE 2.3: SITE LOCATION WITHIN BETTYSTOWN

2.2 Development Description

2.2.1 The proposal is to develop the site for the purpose of a public library and lifeguard station.

2.3 Site Access for Construction Traffic

2.3.1 All delivery and waste vehicles will approach and depart the site via Seaview Terrace and through its junction with the R150 and R151.

2.3.2 It is envisaged that the majority of delivery and waste vehicles approaching and departing the site will travel to and from the west and connect to Drogheda via the R150.

2.3.3 The R150 is the most direct route to Drogheda where the majority building materials will likely be sourced.

2.3.4 Concrete could be sourced from the Roadstone in Mullaghacrone, Co Louth. Concrete delivery vehicles will likely access the site from the west or south. Refer to **Figure 2.4** for details.



FIGURE 2.4: CONCRETE AGITATOR ROUTES TO BETTYSTOWN

2.4 Car / Pedestrian / Cycle Access for Construction Personnel

2.4.1 Car / van access to the site will be via Seaview Terrace.

2.4.2 Pedestrian will also access the site via Seaview Terrace.

2.4.3 Secure cycle parking facilities will be provided for the construction personnel.

2.5 Construction Personnel

2.5.1 It is envisaged that there will be approximately 10 construction personnel on site on a daily basis during peak construction activity. On occasions, this may increase to approximately 15, depending on the individual activities taking place on any particular day.

2.5.2 Full details will be provided in the final version of the CTMP once known.

2.6 Proposed Parking

2.6.1 It is envisaged that part of the public car park / service road will be utilised for the Contractors compound. It is expected that up to six staff parking spaces will be provided on-site.

2.6.2 Assuming that all construction personnel travel via car / van and adopting an occupancy factor of 2.0, it is estimated that the site will generate a demand for up to five (5) parking spaces during normal construction activity.

2.6.3 Occasionally, the parking demand may increase to approximately eight (8) spaces.

2.6.4 The compound's provision (six spaces) will therefore meet the demand during normal construction activity. On the occasional busier period, the overflow (approximately two parking spaces) will be met by public on and off-street parking.

2.6.5 Further details will be provided in the final version of the CTMP once known.

2.7 Construction Site Layout

2.7.1 It is envisaged that the Contractors compound will be partially located on the car park / service road, in front of the subject site.

2.7.2 During construction, the service road will be closed to general traffic. Pedestrians will retain access to the neighbouring commercial properties (Centra Supermarket and Macari's Takeaway) and a delivery bay will be provided to service these premises. Refer to **Figure 2.5** for preliminary details.

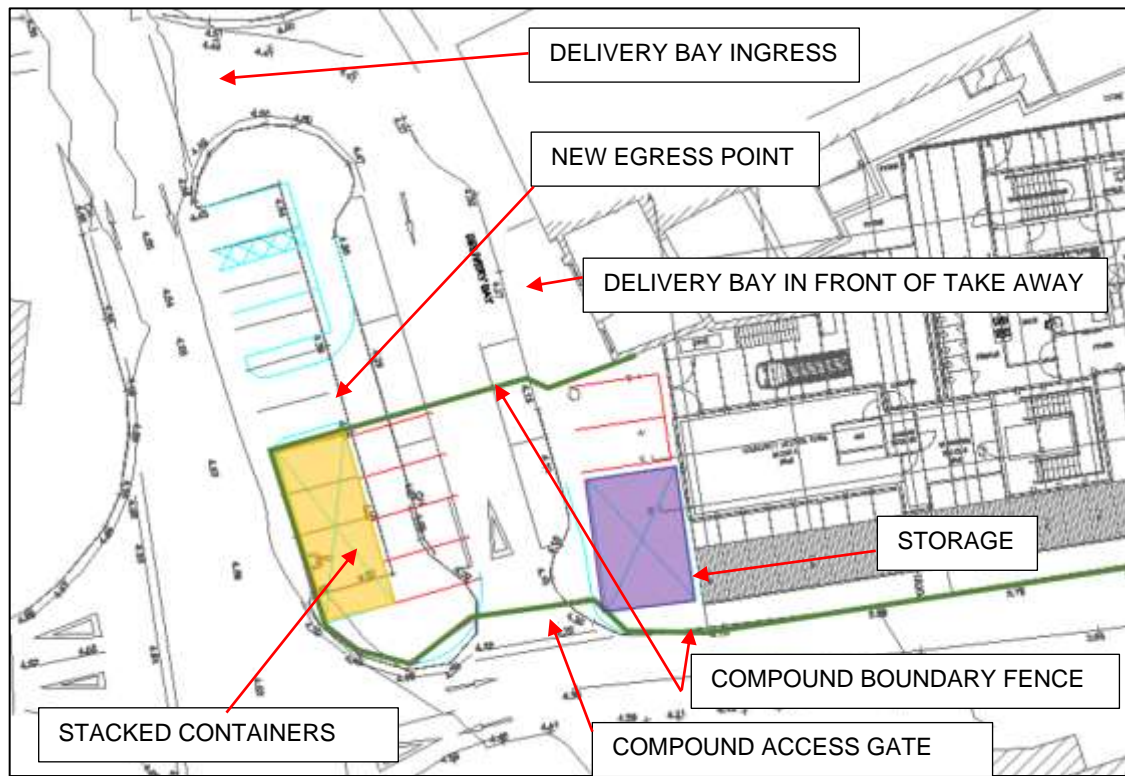


FIGURE 2.5: ENVISAGED SITE COMPOUND

- 2.7.3 The electric vehicle charging point, located adjacent to the car park / service road, will be removed and a temporary service road egress will be constructed to the R151.
- 2.7.4 The Contractors compound will contain welfare facilities, Construction Management Team (CMT) offices, storage areas for the contractors and sub-contractors, together with parking (six bays) for the construction personnel.
- 2.7.5 Just-in-time ordering and off-site pre-fabrication will be used where possible to limit the storage space required on site.
- 2.7.6 A wheel wash will not be provided, however vehicles leaving the site will be inspected and the road will be swept clear on a daily basis. Refer to Section 4.7 for further details.

2.8 Construction Programme

- 2.8.1 It is not possible to provide a definitive construction programme at this application stage. The programme will be dependent on a number of factors such as the detailed design, quantities of materials and development phasing which themselves could be influenced by any planning conditions imposed on the subsequent consent.
- 2.8.2 A detailed construction programme will form part of the final CTMP which will be issued prior to commencement of construction. The appointed construction contractor will be required to prepare the final CTMP.

2.8.3 Notwithstanding the above, it is envisaged that construction will take approximately 12 months to complete.

3.0 CONSTRUCTION TRAFFIC GENERATION

3.1 General

- 3.1.1 As with the construction programme, it is not possible to provide complete details of construction traffic volumes at this planning stage. Further detailed design work, programming and measurement of quantities will be required between the grant of planning permission and commencement of construction. These factors potentially have an influence on the number of deliveries to the site and the number of waste removals from the site.
- 3.1.2 A detailed assessment of construction traffic volumes will form part of the final CTMP which will be issued prior to the commencement of construction. The appointed construction contractor will be required to prepare the final CTMP.
- 3.1.3 All construction traffic will access the site via Seaview Terrace and through its junction with the R150 and R151. Beyond this, it is envisaged that the bulk of construction traffic will approach / depart from / to either the west (R150 west approach) or south (R150 south approach). Fewer vehicle movements are expected from / to the north (R151).

3.2 Scheduling of Deliveries

- 3.2.1 The contractor's Construction Traffic Manager will schedule all deliveries to the site. This will be carried out to prevent clashes in delivery times and prevent congestion on the public road network.
- 3.2.2 Deliveries will be made during typical working hours which are 08:00 to 17:00 Monday to Friday and 08:00 to 13:00 on Saturday. However, as mentioned previously, deliveries will be scheduled to avoid the main traffic peak hours to minimise any potential impacts.
- 3.2.3 Further details will be provided in the final version of the CTMP once known.

3.3 Travel Options for Construction Personnel

- 3.3.1 It is envisaged that construction personnel will travel to and from the site by private car / van or by walking, cycling or public transport. Car / van sharing will be encouraged amongst construction personnel when travelling to and from the site. This is already a standard practice amongst many specialist contracting companies.
- 3.3.2 Pedestrians will access the site via the existing footway network. The site is centrally located and therefore, some construction personal residing in the town will access the site by foot.
- 3.3.3 Cycle parking will be provided within the contractor's compound with the whole of Bettystown and Laytown and much of the surrounding area being within an appropriate 5km cycling distance.

- 3.3.4 Bus Routes D1, D2 and 910 can be accessed from the town centre. Routes D1 and D2 (Drogheda to Laytown) operate to a 30-minute frequency from Monday to Sunday and Route 910 (Drogheda and UCD's Belfield Campus) operates to a mixture of 30-minute and one-hour frequencies on a weekday with reduced services available on a Saturday and Sunday.
- 3.3.5 Route 912 can be accessed outside Tesco. The Route (Drogheda - Connolly Railway Station) operates to a mixture of 30-minute and one-hour frequencies on a weekday with reduced services available on a Saturday.
- 3.3.6 The availability of these non-car travel options will be highlighted to the construction personnel at their initial site briefing / induction sessions.

3.4 Construction Traffic Generation

- 3.4.1 HGV movements are unlikely to exceed approximately 5 – 10 arrivals and departures on a typical day, with these movements likely to be distributed evenly throughout the day.
- 3.4.2 As discussed in Section 2.5, the average number of construction personnel is estimated to be between 10 and 15. Assuming a vehicle occupancy factor of 2.0, this would equate to between five and eight vehicle arrivals at the start and between five and eight vehicle departures at the end of the working day. Additional movements may also occur during lunch breaks or similar.
- 3.4.3 Further details will be provided within the final version of this CTMP prior to commencement of construction.

4.0 CONSTRUCTION TRAFFIC MANAGEMENT PLAN

4.1 General

- 4.1.1 In order to construct the proposed development, it is important that construction traffic and traffic on the existing road network are managed to maximise construction efficiency and safety while minimising risk, inconvenience and nuisance to the public. This will be achieved through careful management, programming and co-ordination of all works and traffic accessing the site.
- 4.1.2 The appointed construction contractor will be required to appoint a Construction Traffic Manager. The Construction Traffic Manager will be required to coordinate and schedule all deliveries to the site, ensure that roadways are kept clear of mud and debris, advise haulage contractors on routes and adhere to good traffic management principles.

4.2 Hours of Working

- 4.2.1 The site working hours are expected to be 08:00 to 17:00 on weekdays and 08:00 to 13:00 on Saturday. No works will be undertaken on Sundays or Bank Holidays, without the consent of the planning authority.
- 4.2.2 In order to maintain these working hours, the Contractor(s) may require a period of up to half an hour before and after normal working hours for preparation and close down activities. This will not include operation of plant or machinery nor will it include import or export of materials to or from the site.
- 4.2.3 In exceptional circumstances, for safety and operational reasons, it may be necessary to work outside of these hours. Should this be required, the hours and duration of work will be subject to consultation with the County Council and kept to a minimum.
- 4.2.4 In certain circumstances, it may be required to arrange deliveries outside of the normal site working hours.

4.3 Access and Egress

- 4.3.1 Refer to Section 2.3 for details.
- 4.3.2 A swept path assessment has been prepared (refer to Appendix A) and confirms that a typical construction delivery vehicle can reverse into the Contractors compound, aided by a banksman, in a safe and satisfactory manner.

4.4 Proposed HGV Routes

- 4.4.1 Again, refer to Section 2.3 for details.

4.5 Traffic Management Principles

4.5.1 To minimise the impact of HGV movements on the existing road network the following construction traffic management principles will be observed:

- To minimise the impact that the construction works will have on local residents and surrounding environment, the Contractor will be required to appoint a Construction Traffic Manager;
- The Construction Traffic Manager will be responsible for ensuring all delivery vehicles to and from the site are managed efficiently and reduce nuisance or unnecessary disruption to the operation of the existing road network;
- The Construction Traffic Manager role will also include advising haulage contractors and their drivers of the most appropriate route to follow when approaching the site in particular providing advice on local width and weight restrictions;
- The Construction Traffic Manager will ensure traffic management principles and the mitigation measures are implemented and that the public road is kept clear of mud and debris through ensuring that loads are suitably sheeted and secured and undertaking regular street cleaning as necessary; and
- To assist the Construction Traffic Manager, a Banksman will be stationed at the main site access gates to directly control HGV movements as they arrive and depart the site. The role will include ensuring loads are appropriately sheeted, ensuring vehicles have passed through the wheel wash and mud is not deposited on the public road, ensuring drivers follow any informal vehicle routings within the site, addressing any immediate issues raised by the public and supervising turning movements in and out of the site.

4.5.2 The final Construction Traffic Management Plan will be incorporated as part of the overall Health and Safety policy for the site.

4.6 Signage

4.6.1 Traffic management signage will be erected by the contractor in accordance with the final CTMP, and as agreed with Meath County Council.

4.6.2 Signage will include construction site entrance warning signs and HGV routing signage. All signage shall be provided in accordance with the Department of Transport's Traffic Signs Manual, Chapter 8 – Temporary Traffic Measures and Signs for Roadworks.

4.7 Wheel Wash

4.7.1 A formal wheel wash cannot be provided. Nevertheless, HGVs exiting the site will be inspected to ensure that vehicle wheels are as clean as possible.

4.8 Road Cleaning

4.8.1 The Construction Traffic Manager will ensure traffic management principles and the mitigation measures are implemented and that the public road is kept clear of mud and debris through ensuring that loads are suitably sheeted and secured and undertaking regular street cleaning as necessary.

4.9 Road Closure / Abnormal Loads

4.9.1 At this time, it is not envisaged that abnormally large or heavy loads will be required during the construction process. If they are subsequently required, they will be coordinated with Meath County Council and the relevant authorities.

4.10 Additional Mitigation Measures

4.10.1 In order to reduce the impact of construction works on the surrounding area, the following measures will be introduced and retained through the course of the construction works:

- No vehicles idling – Where possible, on-site vehicles will be encouraged to turn off their engines when parked, waiting to unload or when not in use. This will reduce the noise impact on the surrounding area and will result in lower vehicle emissions.

5.0 IMPLEMENTATION

5.1 Communication

- 5.1.1 The appointed contractor shall communicate with the local authority and emergency services throughout the construction phase.
- 5.1.2 The contractor shall ensure that all submissions required under the Planning Permission are provided to the local authority and any other body required under the Planning Permission.
- 5.1.3 The contractor shall liaise with the other construction projects in the area to avoid conflicts with deliveries, road closures or similar. The contractor will also liaise with local authority and community representatives about community events that may be impacted by the proposed traffic management measures.
- 5.1.4 The Construction Traffic Manager's contact details will be provided to Meath County Council and be available at the site entrance.

5.2 Emergency Procedures

- 5.2.1 The contractor shall be required to appoint a safety officer for the site and comply with all relevant Safety and Health Legislation, including the provision of first-aiders.
- 5.2.2 The contractor shall provide necessary contact information for responsible personnel to the local authority and emergency services.

5.3 Enforcement

- 5.3.1 All personnel working on site shall be required to adhere to the Final CTMP. An information session on the responsibilities of contractors, as set out in the Final CTMP, shall be included in the site induction programme.

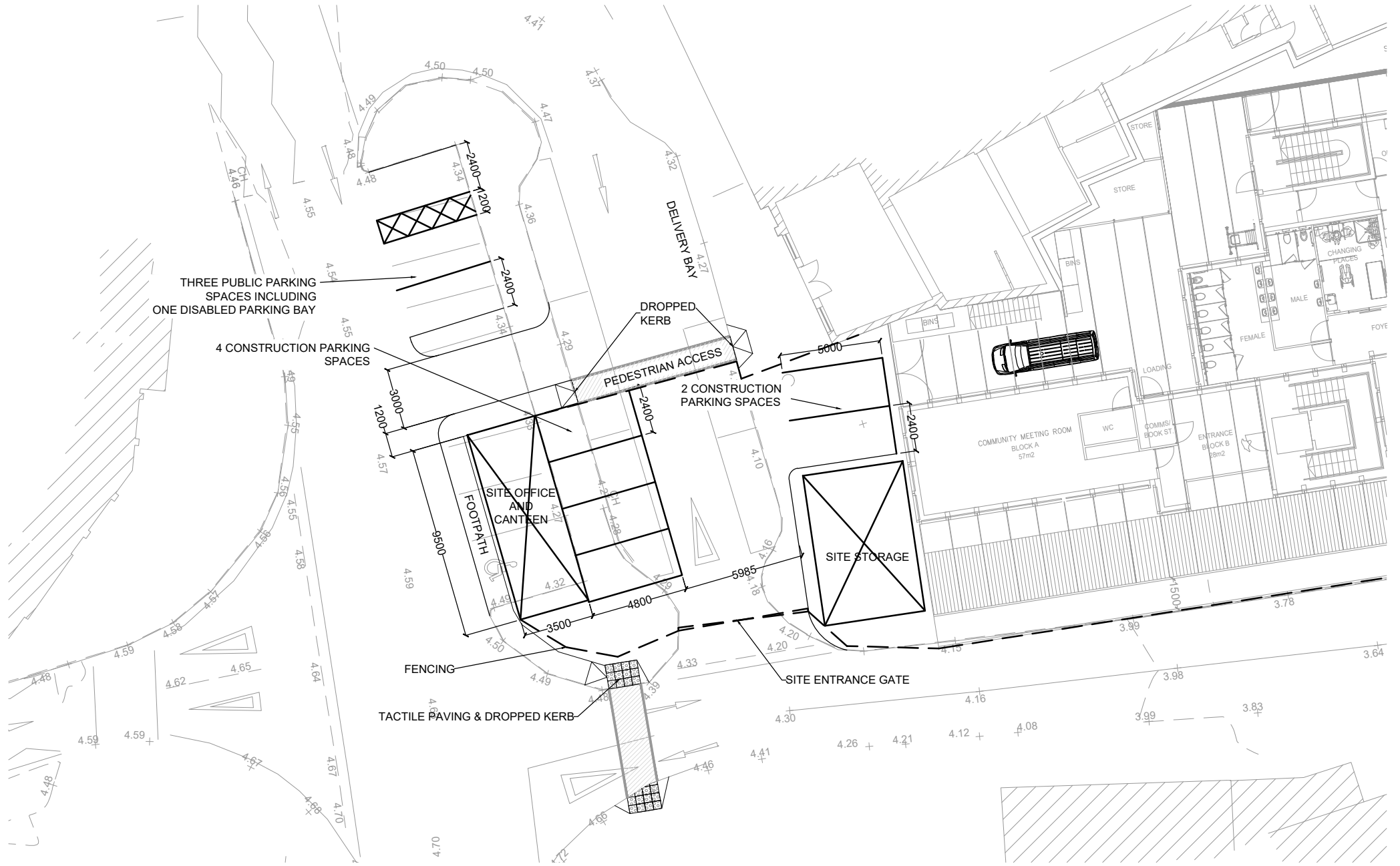
6.0 CONCLUSION

6.1 General

- 6.1.1 This outline Construction Traffic Management Plan (CTMP) sets out the basis for mitigating the impacts of the construction related traffic from the proposed library development in Bettystown, County Meath. There are sufficient vehicle routing and delivery scheduling options available to mitigate the traffic impacts of the construction phase.
- 6.1.2 The site compound will be located on the car park / service road. The compound will be set up to accommodate delivery vehicles safely reversing into the site whilst maintaining six (6) parking spaces for construction personnel. Refer to Figure 2.5 for details.
- 6.1.3 The appointed construction contractor will be required to prepare the final CTMP, at which point greater detail will be available on methods of construction, volumes of construction materials and likely traffic movements associated with the construction. The final CTMP will also demonstrate compliance with any conditions set out in the Planning Permission.
- 6.1.4 The appointed construction contractor will be required to appoint a Construction Traffic Manager who will be required to coordinate and schedule all deliveries to the site, ensure that roadways are kept clear of mud and debris, advise haulage contractors of appropriate routes and ensure adherence to good traffic management principles.

APPENDIX A

Figures



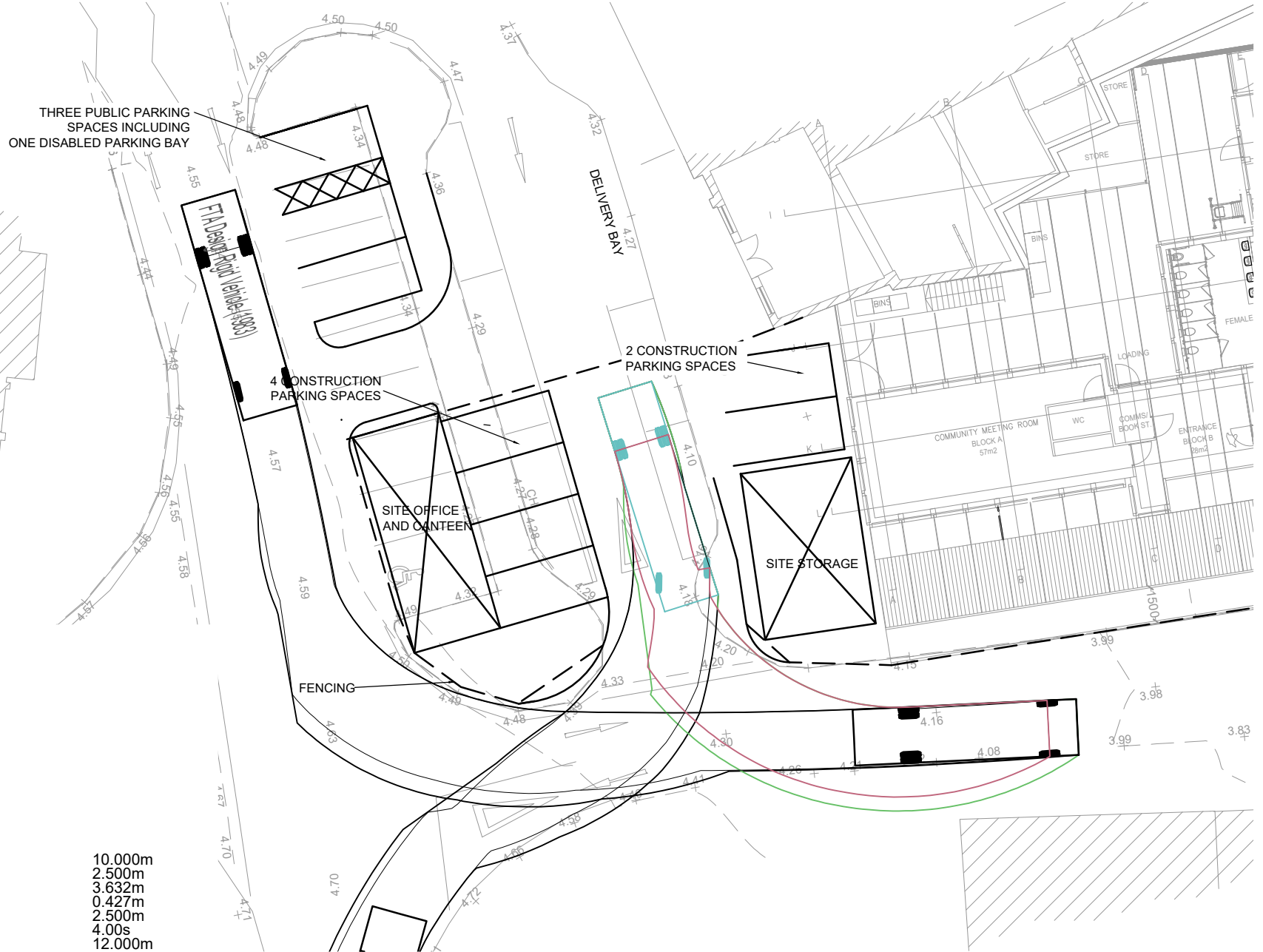
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 Project: BETTYSTOWN LIBRARY
 SEAVIEW TERRACE, BETTYSTOWN

Drawing Title: PROPOSED SITE COMPOUND
 Project No: 191018
 Drawing No: 191018-CTMP-SK01
 Rev: PL1

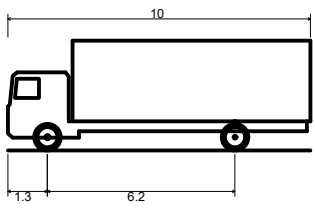
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PL1	25.06.20	PLANNING ISSUE	MT	MJP	MJP

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VEHICLE PROFILE



FTA Design Rigid Vehicle (1983)
 Overall Length 10.000m
 Overall Width 2.500m
 Overall Body Height 3.632m
 Min Body Ground Clearance 0.427m
 Track Width 2.500m
 Lock-to-lock time 4.00s
 Curb to Curb Turning Radius 12.000m



Client: MEATH COUNTY COUNCIL
 Project: BETTYSTOWN LIBRARY
 SEAVIEW TERRACE, BETTYSTOWN

Drawing Title: PROPOSED AUTOTRACK LAYOUT
 Project No: 191018
 Drawing No: 191018-CTMP-SK02
 Rev: PL1

Drawn: MT
 Date: 29/04/20
 Checked: MJP
 Scale: 1:250

PLANNING			Drawn	Checked	Approved
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PL1	25.06.20	PLANNING ISSUE	MT	MJP	MJP

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